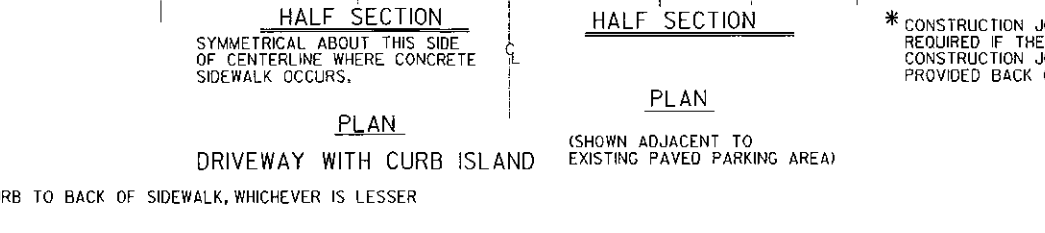
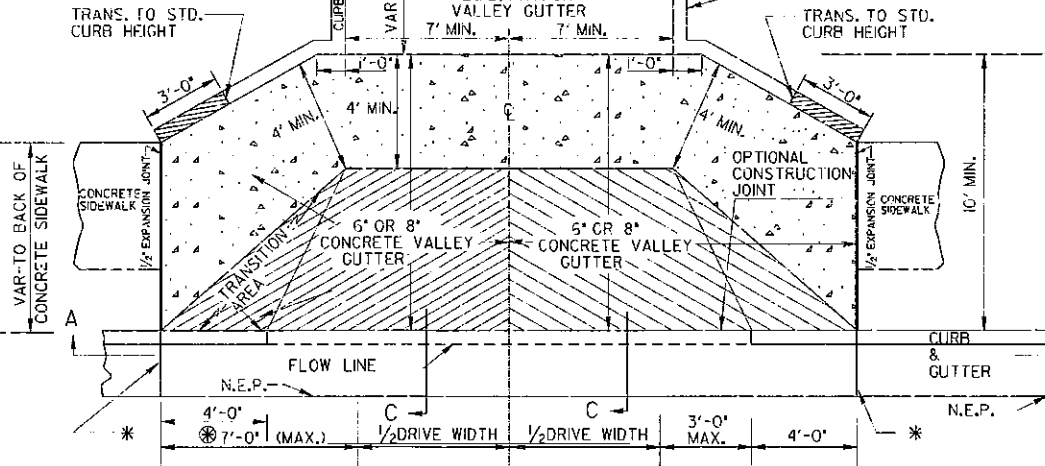
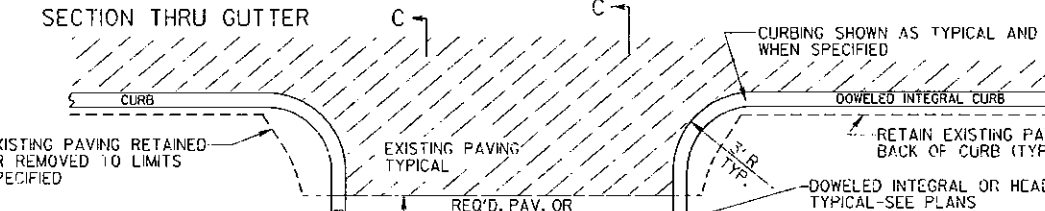
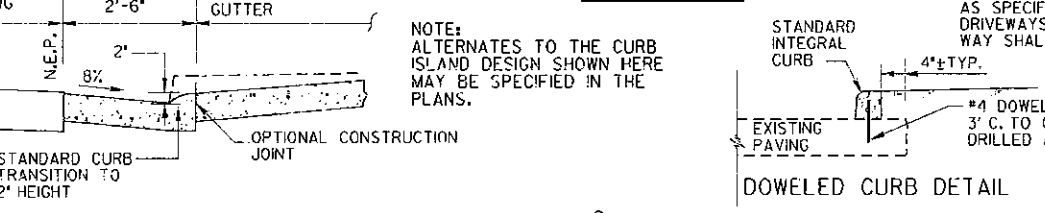
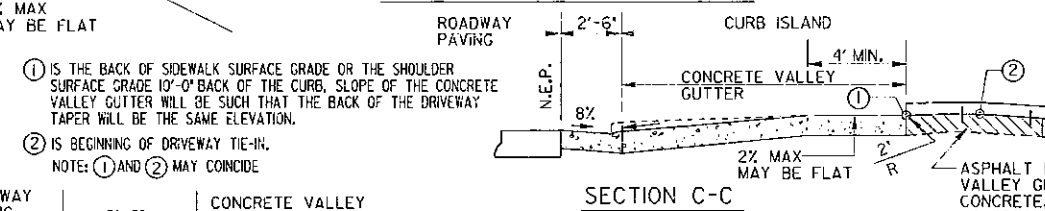
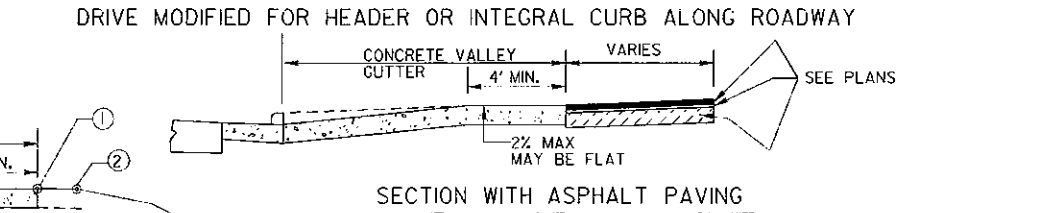
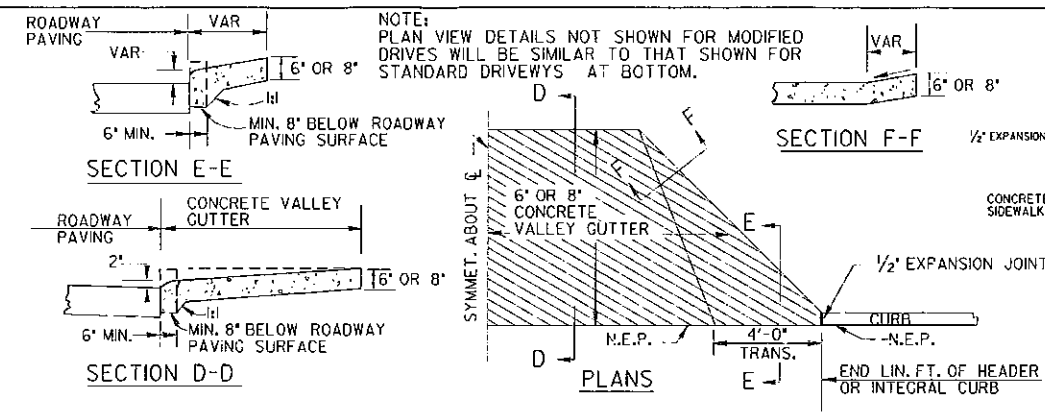


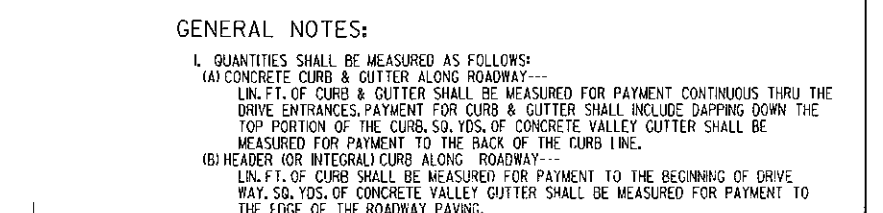
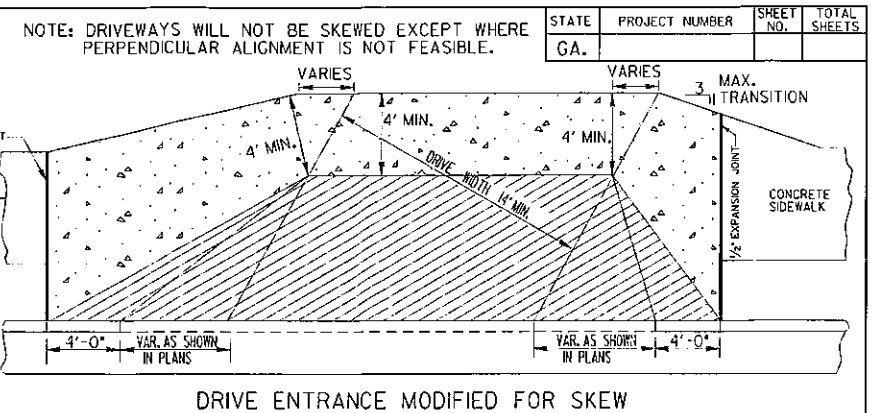
* CONSTRUCTION JOINT NOT REQUIRED IF THE OPTIONAL CONSTRUCTION JOINT IS PROVIDED AT BACK OF THE CURB.

⊗ 7'-0" (MAX.)

⊗ 7'-0" OR DISTANCE EQUAL TO THAT FROM BACK OF CURB TO BACK OF SIDEWALK, WHICHEVER IS LESSER



NOTE: PLAN VIEW DETAILS NOT SHOWN FOR MODIFIED DRIVES WILL BE SIMILAR TO THAT SHOWN FOR STANDARD DRIVEWAYS AT BOTTOM.



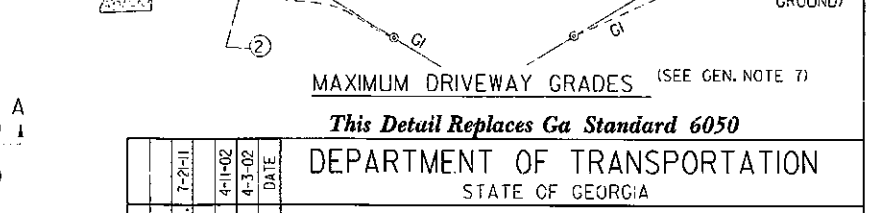
- QUANTITIES SHALL BE MEASURED AS FOLLOWS:
(A) CONCRETE CURB & GUTTER ALONG ROADWAY---
LIN. FT. OF CURB & GUTTER SHALL BE MEASURED FOR PAYMENT CONTINUOUS THRU THE DRIVE ENTRANCES. PAYMENT FOR CURB & GUTTER SHALL INCLUDE DAPPING DOWN THE TOP PORTION OF THE CURB, SO YDS. OF CONCRETE VALLEY GUTTER SHALL BE MEASURED FOR PAYMENT TO THE BACK OF THE CURB LINE.
(B) HEADER (OR INTEGRAL) CURB ALONG ROADWAY---
LIN. FT. OF CURB SHALL BE MEASURED FOR PAYMENT TO THE BEGINNING OF DRIVE WAY, SO YDS. OF CONCRETE VALLEY GUTTER SHALL BE MEASURED FOR PAYMENT TO THE EDGE OF THE ROADWAY PAVING.
- N.E.P. IS DEFINED AS THE POINT WHERE THE ROADWAY PAVING MEETS THE CURB & GUTTER, OR HEADER CURB, OR FACE OF THE INTEGRAL CURB.
- DRIVES RECONSTRUCTED SHALL BE REPLACED IN THE KIND, I.E. ASPHALT FOR ASPHALT, CONCRETE FOR CONCRETE, AND PAVED TO THE RIGHT OF WAY LINE.
- SEE STANDARD 9032-B FOR DETAILS OF CONCRETE CURB & GUTTER, HEADER CURBS AND DOWELED INTEGRAL CURBS.
- WIDTHS OF COMMERCIAL DRIVEWAYS SHALL COMPLY WITH CURRENT RULES AND REGULATIONS FOR DRIVEWAY AND ENCROACHMENT CONTROL. WIDTHS OF RESIDENTIAL NON-COMMERCIAL DRIVEWAYS SHALL BE AS SPECIFIED IN THE PLANS.
- THE SLOPE OF THE "TRANSITION AREA" OF THE CONCRETE VALLEY GUTTER SHALL NOT BE STEEPER THAN 8% (2:1) WHERE SIDEWALKS ARE LOCATED.
- MAXIMUM DRIVEWAY GRADES SHOWN BELOW ARE INTENDED FOR RESIDENTIAL DRIVEWAYS WHERE FLATTER GRADES ARE NOT FEASIBLE. GRADES FOR COMMERCIAL DRIVEWAYS OR FOR TRUCKS SHALL NOT BE GREATER THAN 12% UNLESS SPECIFIED OTHERWISE.

Guidelines For Usage On Metro Projects

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- IS THE BACK OF SIDEWALK SURFACE GRADE OR THE SHOULDER SURFACE GRADE 10'-0" BACK OF THE CURB. SLOPE OF THE CONCRETE VALLEY GUTTER WILL BE SUCH THAT THE BACK OF THE DRIVEWAY TAPER WILL BE THE SAME ELEVATION.
 - IS BEGINNING OF DRIVEWAY TIE-IN.
- NOTE: (1) AND (2) MAY COINCIDE

V.C.	MAXIMUM GI		V.C.	MAX. ALGEBRAIC GRADE CHANGE	
	CUT	FILL		SAG	CREST
5'	27%	16.67%	5'	2%	25%
10'	28%	27%	10'	25%	36%



This Detail Replaces Ga Standard 6050

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

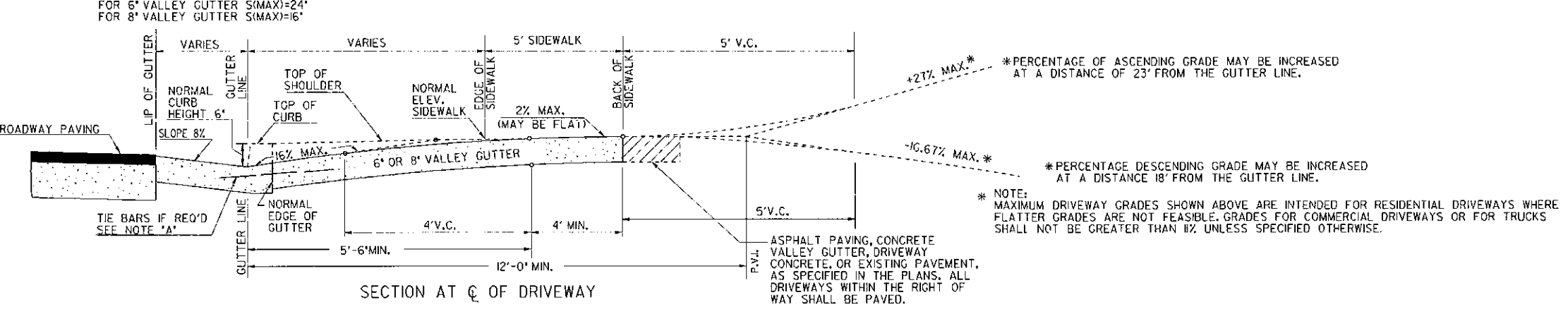
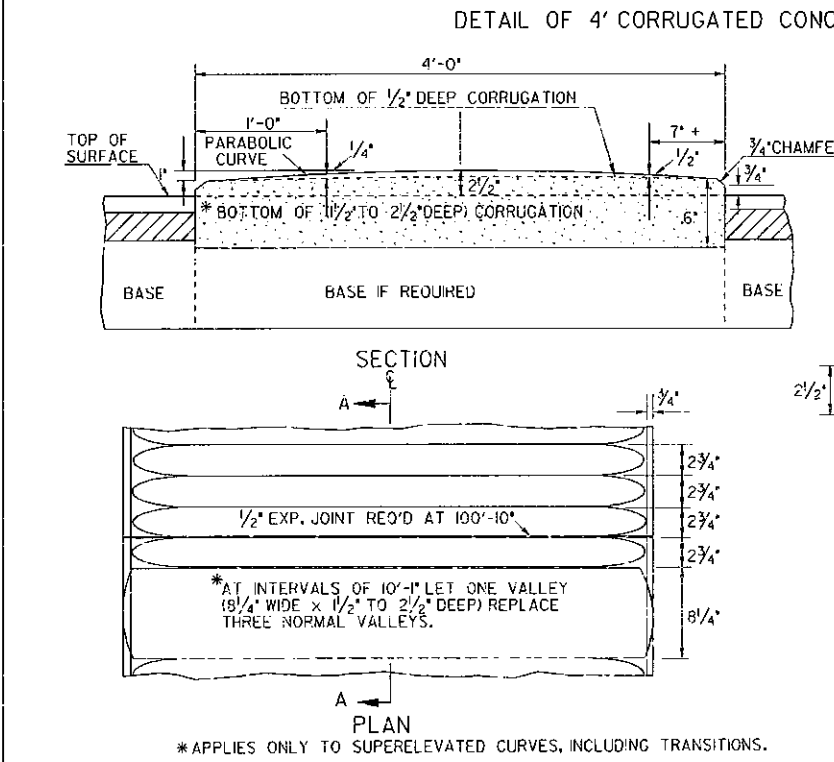
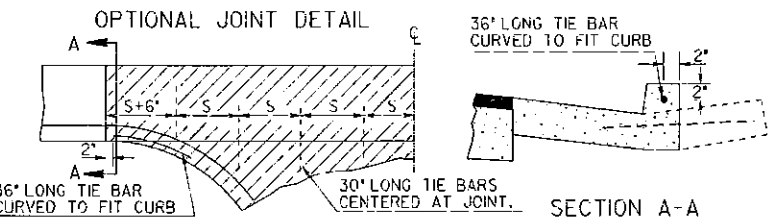
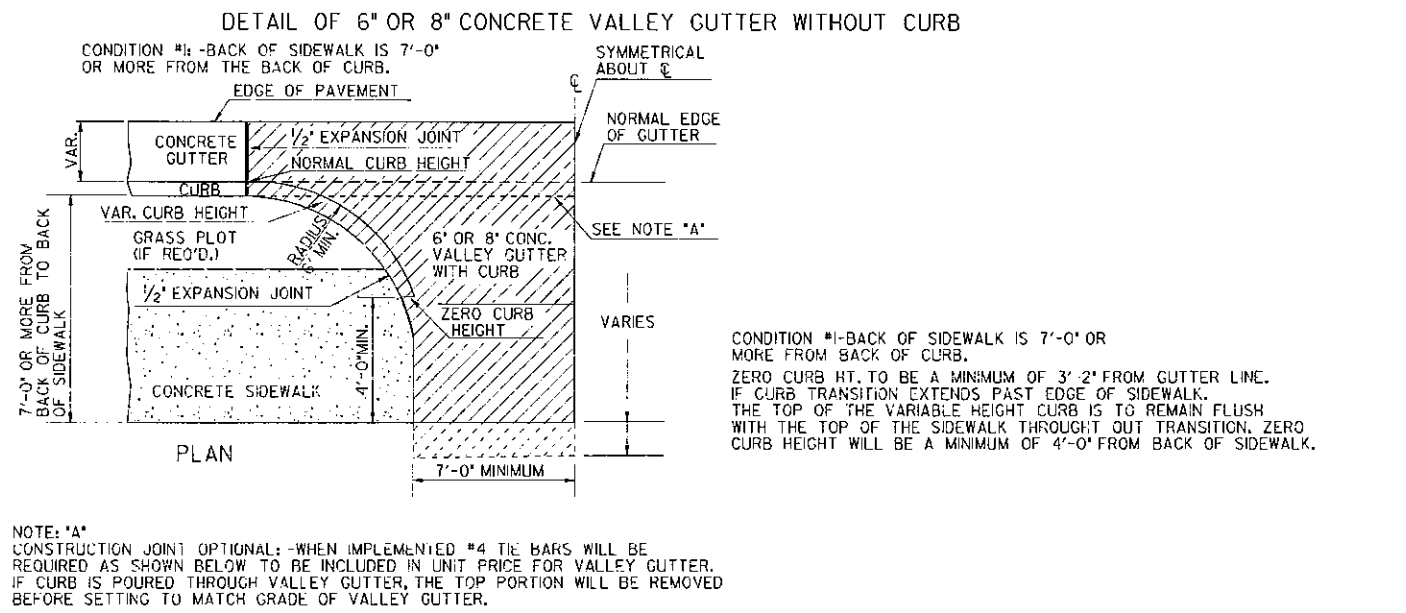
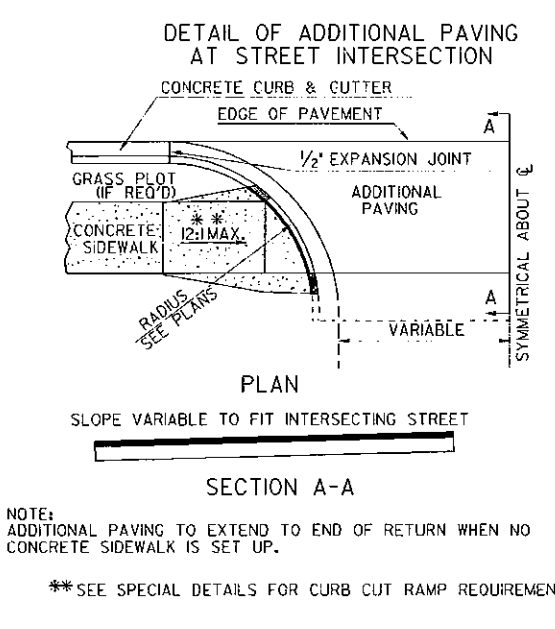
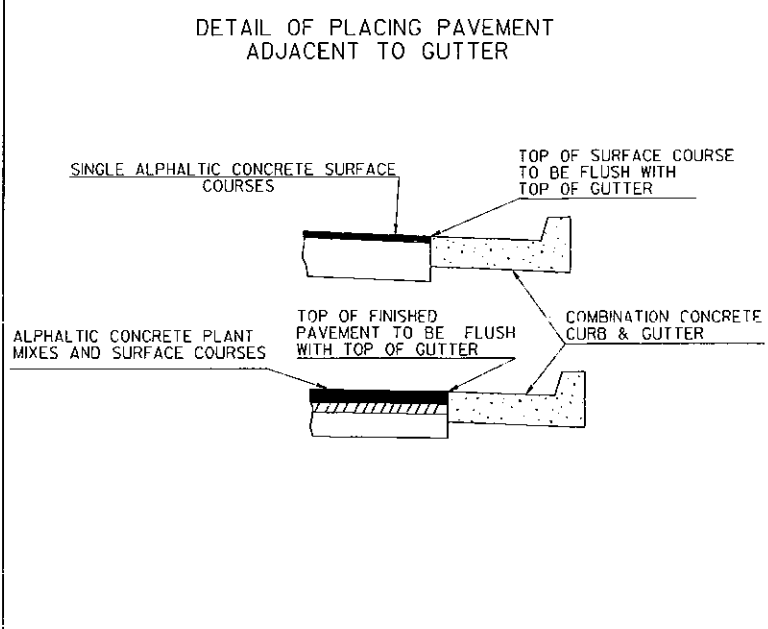
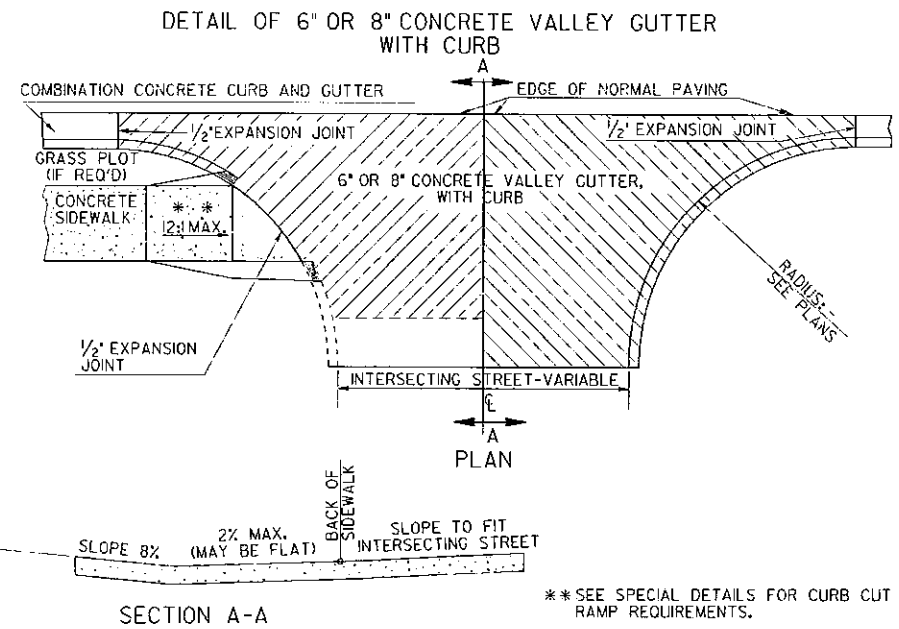
CONSTRUCTION DETAIL
DRIVEWAYS WITH TAPERED
ENTRANCES
CONCRETE VALLEY GUTTERS

NO SCALE

MARCH 12, 2002

NUMBER
A1

REV. PAVEMENT NOTES, REV.	7-2-11	DATE
12" TO 14" MIN. REV. SIDEWALK	4-11-02	REVISION
REVISED	4-3-02	BY
GLO		



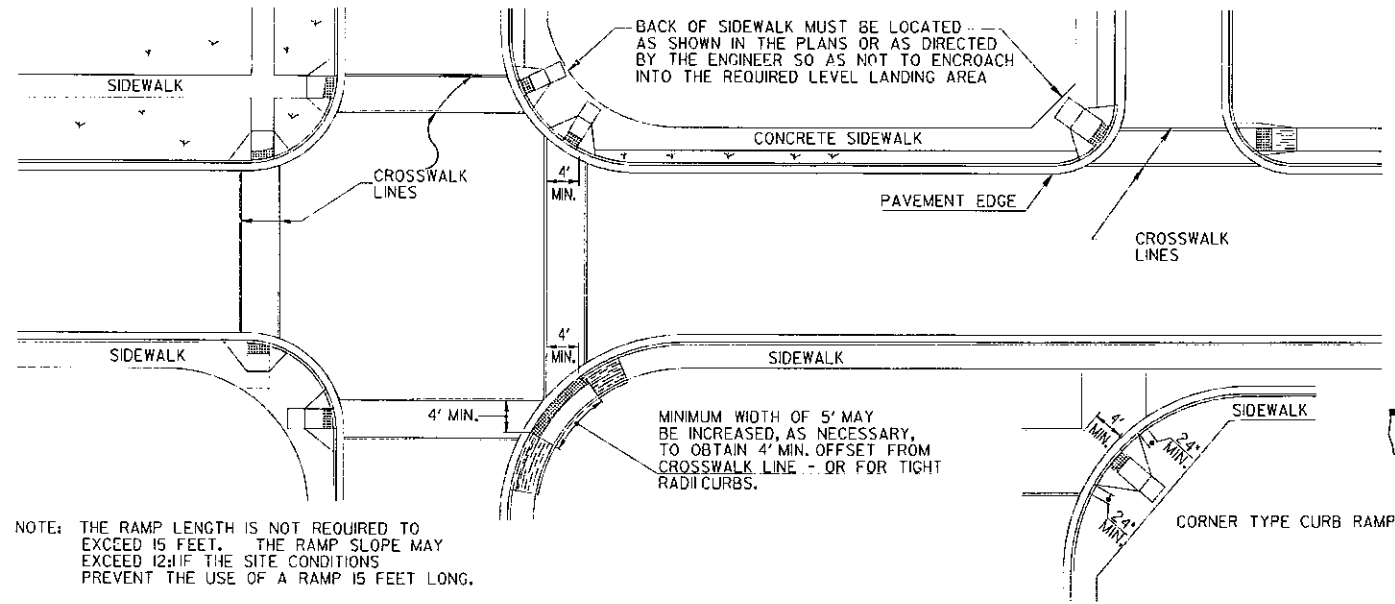
This Detail Replaces Ga Standard 9031J

Guidelines For Usage On Metric Projects

When these details are incorporated into plans and or projects that are being prepared or constructed in metric units, exact or precise conversion to metric units is not required. The dimensions shown that are in feet and inches may be converted to corresponding metric units using the following "Rounded-Off" conversion factors: 1" = 25mm, 4" = 100mm, and 12" = 300mm. All measurement notes that refer to linear feet and square yards shall be interpreted to mean linear meters and square meters.

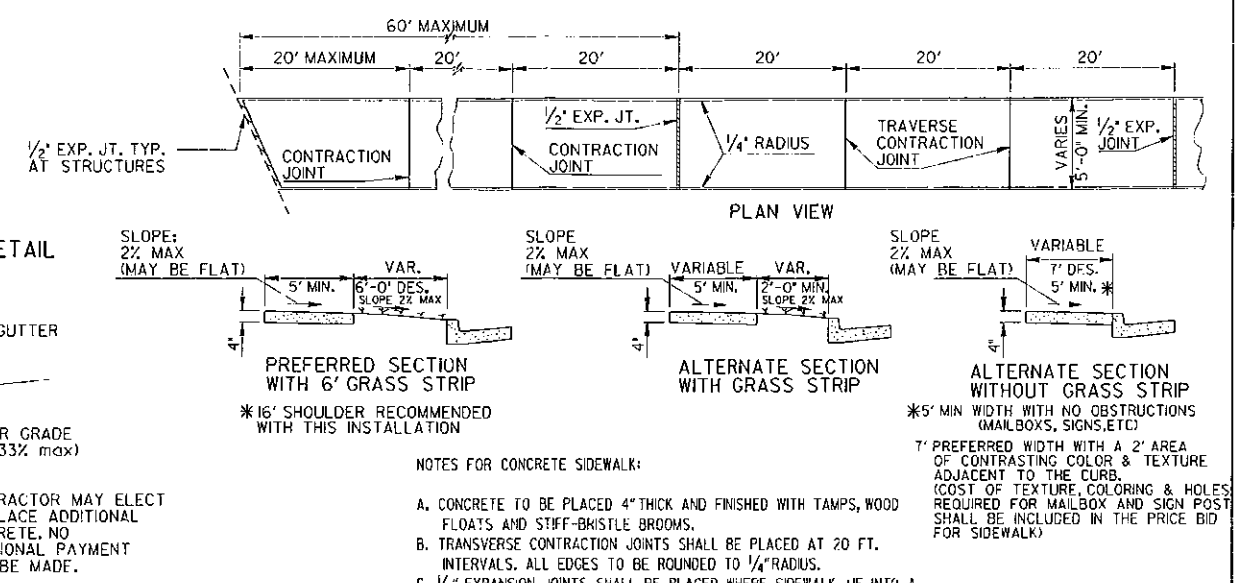
7-2-11		2-21-03		4-1-02		4-3-02		DATE		DEPARTMENT OF TRANSPORTATION	
GLO		GLO		BY		REVISION		NO SCALE		STATE OF GEORGIA	
ADDED PAVEMENT NOTE, REV 7-2-11		REVISED PAVEMENT NOTES		REVISED		REVISED		REVISED		CONSTRUCTION DETAIL	
										CONCRETE VALLEY GUTTER AT STREET INTERSECTION	
										6" OR 8" CONCRETE VALLEY GUTTER AT DRIVE	
										PLACING PAVEMENT ADJACENT TO GUTTER	
										ADDITIONAL PAVING AT STREET INTERSECTION	
										4" CORRUGATED CONCRETE MEDIAN	
										MARCH 12, 2002	
										NUMBER	
										A2	

TYPICAL LOCATIONS FOR CURB CUT RAMPS - PLAN VIEW



NOTE: THE RAMP LENGTH IS NOT REQUIRED TO EXCEED 15 FEET. THE RAMP SLOPE MAY EXCEED 12% IF THE SITE CONDITIONS PREVENT THE USE OF A RAMP 15 FEET LONG.

CONCRETE SIDEWALK DETAILS



NOTES FOR CONCRETE SIDEWALK:

- CONCRETE TO BE PLACED 4" THICK AND FINISHED WITH TAMPS, WOOD FLOATS AND STEEL-BRISTLE BROOMS.
- TRANSVERSE CONTRACTION JOINTS SHALL BE PLACED AT 20 FT. INTERVALS. ALL EDGES TO BE ROUNDED TO 1/4" RADIUS.
- 1/2" EXPANSION JOINTS SHALL BE PLACED WHERE SIDEWALK MEETS INTO A STRUCTURE OR TERMINATE AT CURB, RAMPS OR DRIVEWAYS AND AT 60' INTERVALS.

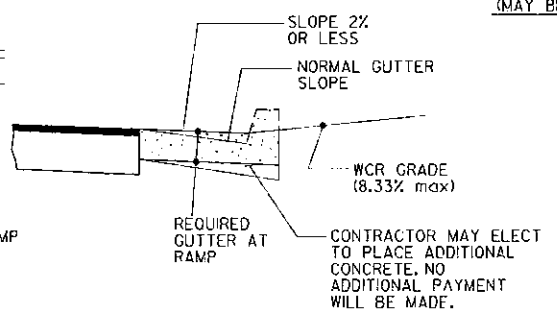
NOTES FOR CURB CUT RAMPS:

- CURB CUT RAMPS WILL BE LOCATED AS FOLLOWS UNLESS PLANS OR CONTRACT SPECIFY OTHERWISE.
 - AT ALL PEDESTRIAN CROSSWALKS WHERE CURB IS CONSTRUCTED OR REPLACED.
 - WHERE THE SIDEWALK, CONCRETE OR UNPAVED, IS INTERRUPTED BY THE CURB AT TURNOUTS OR AT INTERSECTIONS.
 - AT OTHER LOCATIONS SUCH AS HOSPITALS, NURSING HOMES, REST AREAS, ETC., WHERE THE CURB WOULD OTHERWISE BE AN OBSTRUCTION TO THE PHYSICALLY DISABLED.
- RAMPS WILL BE CONSTRUCTED FROM CONCRETE. SPECIFICATIONS FOR RAMPS WILL BE THE SAME AS FOR CONCRETE SIDEWALK. RAMPS SHALL HAVE EITHER A ROUGH OR A TEXTURED FINISH.
- DROP INLETS ARE NOT TO BE LOCATED DIRECTLY IN FRONT OF RAMPS. CATCH BASINS SHOULD BE LOCATED AT LEAST 10 FT. FROM RAMPS WHEN FEASIBLE.
- WHERE RAMPS ARE LOCATED IN RADIUS, THE DIMENSIONS SHOWN FOR RAMP WIDTHS AND TAPERS ARE MEASURED PERPENDICULAR TO THE RAMP AND NOT ALONG THE CURVE.
- WHERE UTILITY STRUCTURES CONFLICT, WHERE SIDEWALK GEOMETRY VARIES, AT SKEWED INTERSECTIONS, OR IN OTHER SPECIAL CASES, THE RAMP DESIGNS MAY BE MODIFIED BY THE DESIGNER OR ENGINEER, PROVIDED THAT THE WIDTH REMAINS A MINIMUM OF 48 INCHES, AND NO SLOPE ON THE ACCESSIBLE PART OF THE RAMP IS STEEPER THAN 12:1.
- 10 FT. OF CURB AND GUTTER WILL INCLUDE THE TRANSITIONED CURB IN FRONT OF RAMPS. SO. YDS. OF CONCRETE SIDEWALK AND CONCRETE MEDIAN PAVING WILL INCLUDE RAMPS. NO ADDITIONAL PAYMENT WILL BE MADE FOR CURB RAMPS. NO ADDITIONAL PAYMENT WILL BE MADE FOR SAWING AND REMOVING EXISTING SIDEWALK OR CURB WHERE NECESSARY FOR RAMP CONSTRUCTION.
- WHEN A CURB RAMP IS PLACED ON EXISTING PAVEMENT, THE PAVEMENT SHALL BE REMOVED TO PROVIDE A MINIMUM THICKNESS OF 3 INCHES OF CONCRETE AT ALL LOCATIONS. NO SEPARATE PAYMENT WILL BE MADE FOR REMOVAL OF THE PAVEMENT.
- DETECTABLE WARNING SURFACES ARE REQUIRED ON ALL INTERSECTIONS WITH PUBLIC STREETS, SIGNALIZED COMMERCIAL DRIVEWAYS, AND COMMERCIAL DRIVEWAYS WITH AN AADT OF 25 VPD.

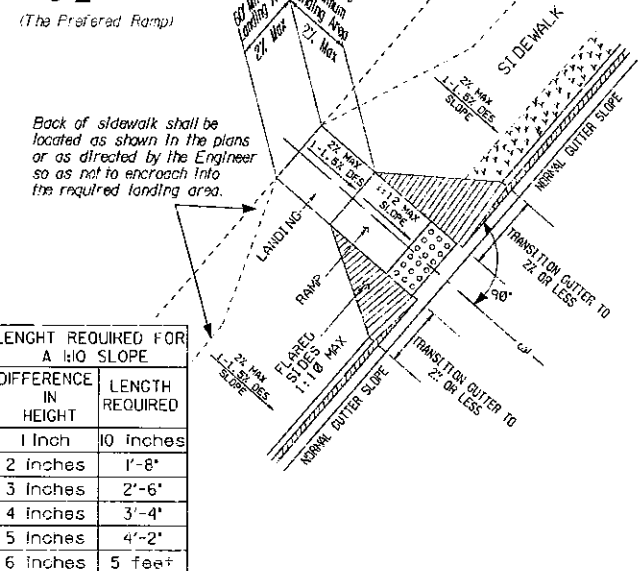
This Detail Replaces Ga Standard 9031W

Guidelines For Usage On Metric Projects
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GUTTER TRANSITION DETAIL

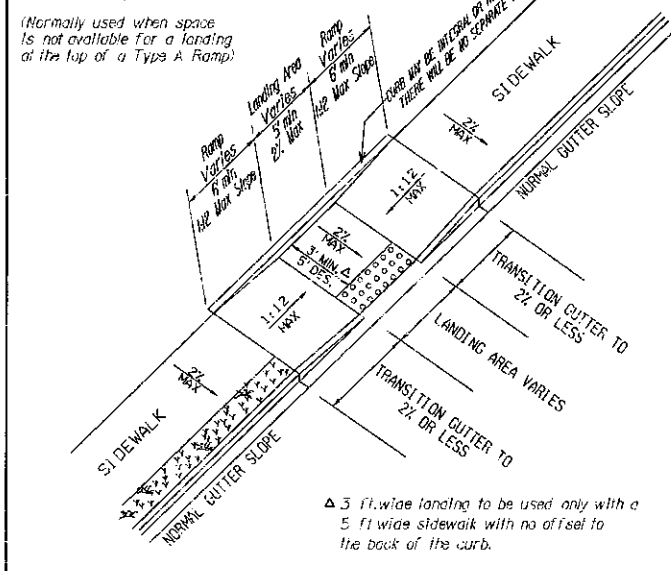


Type A

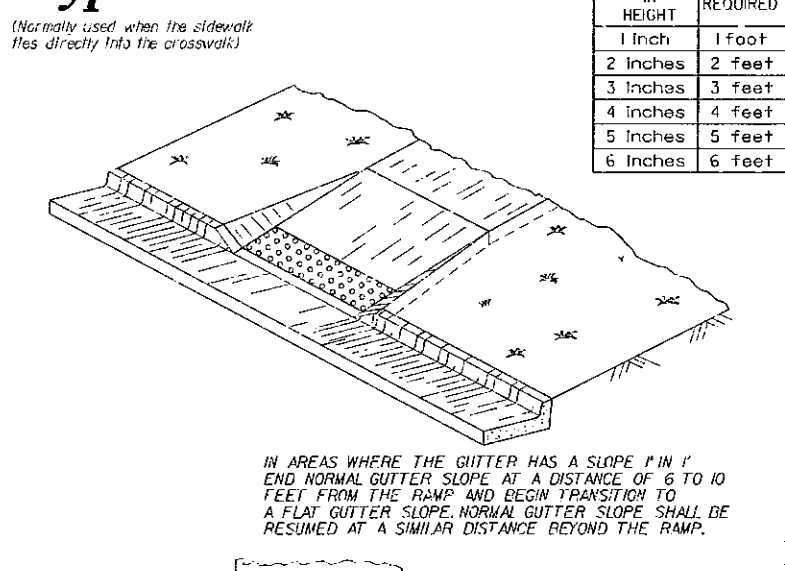


DIFFERENCE IN HEIGHT	LENGTH REQUIRED
1 inch	10 inches
2 inches	1'-8"
3 inches	2'-6"
4 inches	3'-4"
5 inches	4'-2"
6 inches	5 feet+

Type B

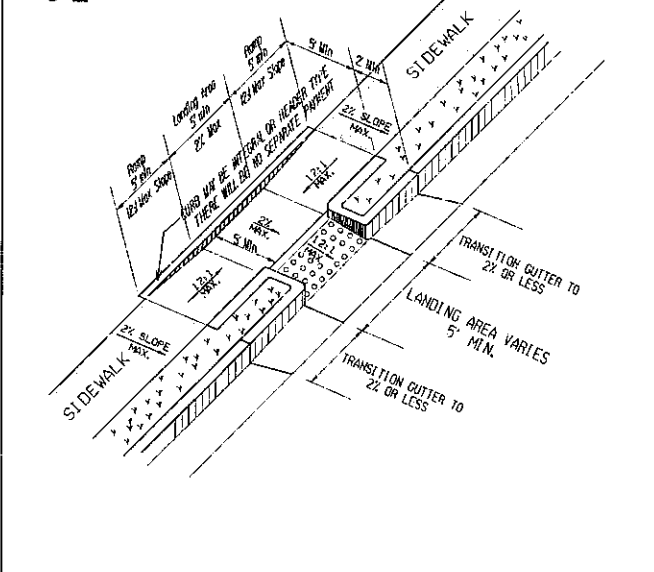


Type D

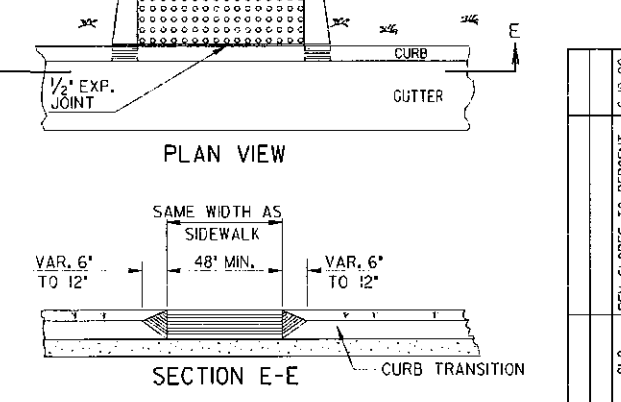
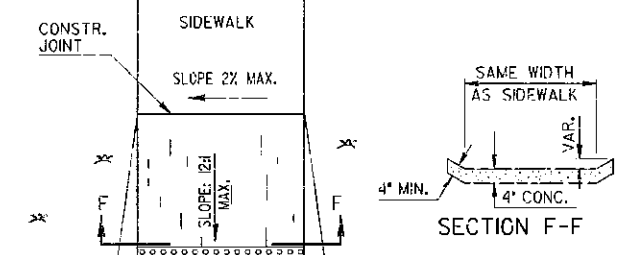
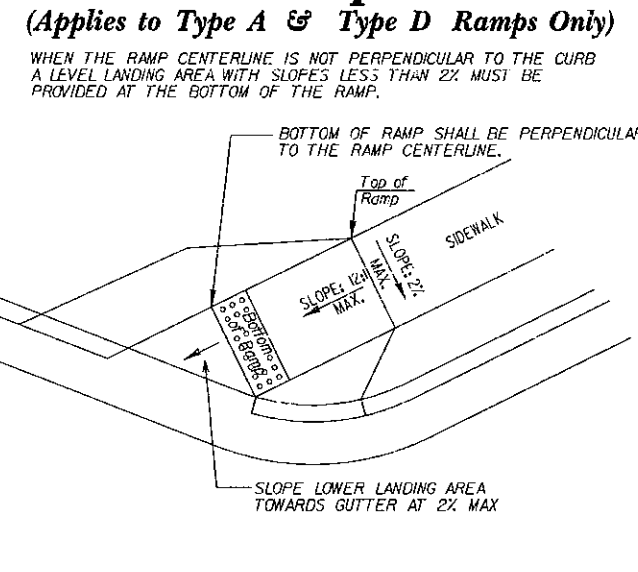


DIFFERENCE IN HEIGHT	LENGTH REQUIRED
1 inch	1 foot
2 inches	2 feet
3 inches	3 feet
4 inches	4 feet
5 inches	5 feet
6 inches	6 feet

Type C



Skewed Ramp Details



REV.	DATE	DESCRIPTION
6-18-05		REV. SLOPES TO PERCENT AND ADDED 12:1 & 10:1 CHART.
5-10-06		REV. TRUNCATED DOMES
2-21-03		REVISED
2-10-03		REVISED
7-29-02		REVISED
5-29-02		REVISED
5-23-02		REVISED
5-13-02		REVISED
4-29-02		REVISED
4-11-02		REVISED
4-3-02		REVISED
3-28-02		REVISED

DEPARTMENT OF TRANSPORTATION
STATE OF GEORGIA

SPECIAL DETAIL
CONCRETE SIDEWALK DETAILS
CURB CUT (WHEELCHAIR) RAMPS

NO SCALE

MARCH 12, 2002

NUMBER
A3